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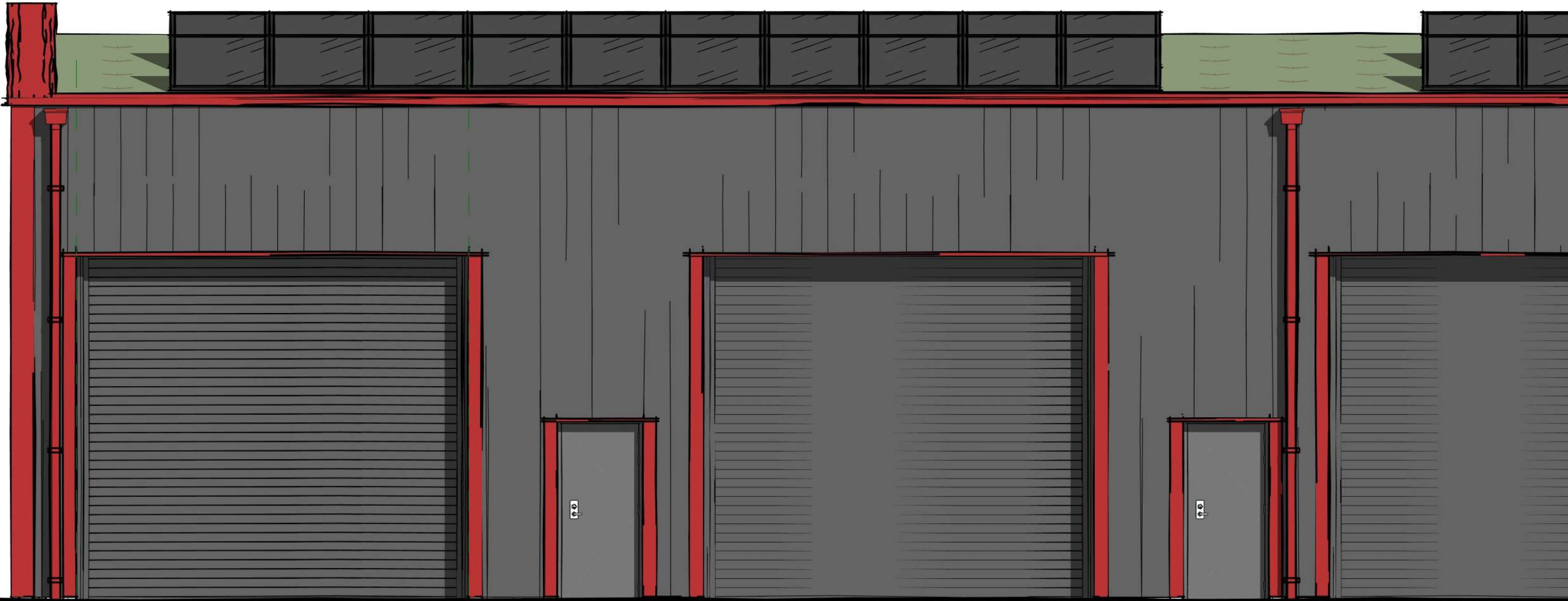
UNIT 12 ATLANTIC TRADING ESTATE BARRY

for
Forklift Specialist

July 2025

DESIGN & ACCESS STATEMENT

3252 (DAS) 001 rev P00



Revisions

P00	Public consultation issue	04/07/2025
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1.0 INTRODUCTION

1.1 Introduction

This Design and Access Statement (DAS) has been prepared by Evans Architectural Ltd on behalf of Forklift Specialists to support a planning application for the rebuilding of 1no industrial building to provide ten individual units with the proposed planning use class of B2. The new development is located on a similar footprint of the previous unit which was destroyed by fire in October 2023.

This document presents an analysis of the site and its surrounding context, site history and an outline of the proposed development. It summarizes a range of issues and opportunities and identifies how these have informed the development proposals.

1.2 The Proposal

The proposal is for planning use class B2 General Industrial which is similar to the previous use of the site. The development proposal is for one large portal frame industrial unit sub-divided to provide ten individual units with shared access, and shared external areas, parking etc.

2.0 CHARACTER

2.1 Site Location & Context

The proposed development site is located at the Northern area of Atlantic Trading Estate which is an established industrial area.

The area of the application site is approximately 4055m² (0.405 hectares).

The development site shares an existing access road from Atlantic Trading Estate with the VoG Resource Recovery Facility.

The application site is enclosed on all sides by security fencing with neighbouring properties to the east, south and west also being industrial in nature. To the north of the site, on the opposite side of the road, is a small area of undeveloped green land to which includes trees and various unmaintained plants, grasses etc.

To the east of the site is the relatively recently constructed VoG Local Authority Resource Recovery Facility.

Slightly further north and west of the site is the Cadoxton River and to the south, the Bristol Channel.



2.2 Context Analysis– Existing Views of The Site

Other than the security fencing that currently encloses the application site there are no permanent buildings or structures remaining on the site. The previous portal frame structure, which stood for a number of years, was destroyed by fire in October 2023 leaving just the original concrete slab and foundations. The shared access from Atlantic Trading Estate is existing and will be retained.

An analysis of the surrounding built context has been undertaken to inform on appropriate scale and form for the proposed development. This analysis included built forms, materials and finishes, key building elements and colour palette. Whilst not exhaustive it is intended to inform an appropriate architectural response for the development proposals.

Existing external finishes are typical of an industrial development and include facing brickwork, sheet cladding in grey, blue or green, white or dark grey windows and doors. Occasionally doors, RWGs and corner trims are highlighted in contrasting colours. To maintain a level of consistency between the buildings, it is proposed to use similar materials and colours for the proposed development.

All the units in the area are very typical industrial units.



Image: Resource Recovery Facility



Image: Vehicle holding Plots to the North.



Image: Above & below, examples of units nearby.



Image: Above & below, examples of units nearby.



2.3 Planning History & Planning Policy

Planning History

Planning Reference 96/00494/FUL

Construction of Industrial/ factory unit at Unit 12, Atlantic Trading Estate, Barry.

Decision: Approved

Planning Reference 2015/00425/FUL

Change of use to a non hazardous waste transfer station.

Decision: Approved

Planning Reference 2016/00762/OBS

A16 Non-hazardous waste physical treatment facility.

Decision: Approved

Planning Reference 2023/00823/FUL

New crossover access way to land at rear of Unit 12F Atlantic Trading Estate.

Decision: Approved

Planning Reference 2022/00167/FUL

New access road from public highway to industrial unit.

Decision: Approved

Planning Reference 2024/00267/FUL

Erect new steel Portal Units.

Decision: Pending

Planning Policy Response

The nature of the development, which seeks to make beneficial use of a currently vacant site, which sits in the centre of industrial development, aligns well with the PPW which promotes the most efficient use of the available land resource.

Due to the location, and position amongst an existing industrial estate, the development site is not suitable for many alternative uses.

The proposal is to re-instate an existing industrial facility that was destroyed in 2023. Many businesses, including the applicant, lost their premises in 2023 and an urgent need is required to re-construct and provide new facilities in order for the remaining businesses to survive and grow. New units will also provide opportunity to attract new and established businesses to the area.

In design terms, the proposal is appropriate for the location and the relevant policy context, as the character and scale of the building are suitable to the site and area with finishes and materials used being suitable and appropriate with the type of development and its intended use, whilst also reflecting those within the area.

2.4 Constraints and Opportunities

The following opportunities and constraints have been identified following a feasibility study.

Constraints

- Site is not suitable for alternative purposes.
- New development must include sustainable drainage design in accordance with SAB.

Opportunities

- Re-development of a vacant brownfield site with limited suitable alternative uses.
- Provision of 10no industrial units which are ideal for established and start-up businesses.
- The existing site provides no contribution to ecology or biodiversity. The proposals incorporate a green roof as part of the sustainable drainage strategy which provides a significant biodiversity enhancement opportunity. Where possible green areas to include various native plants and trees is also proposed.
- Increased employment and local business investment and expansion opportunities.
- With a new development at the Resource Recovery Facility and with new proposals for Unit 15 too, this is an opportunity to renew a generally very tired looking area in the Atlantic Trading Estate. The previous unit on this site stood for 27 years before being destroyed in a fire.



3.0 DESIGN

3.1 Design Brief

The design brief stipulated the following requirements:

- A number of industrial units to fit the site, each with a GIA (gross internal area) of around 170m² incorporating pedestrian access and loading bay doors.
- Continued use of shared access from Atlantic Trading Estate with Resource Recovery Facility.
- External appearance to compliment the existing buildings within Atlantic Trading Estate.
- Suitable parking provision.
- Suitable and safe cycle storage facilities.
- Suitable drainage provision complying with the SAB requirements.

The finishes and external appearance will reflect those at within the Atlantic Trading Estate to give a consistent look and feel to the development.

3.2 Feasibility and Concept Design

The proposed layout for this site was decided in the 1996 planning submission when the original unit was built. The majority of the 2025 proposal will sit on the existing slab– which is the only element remaining after the fire in 2023.

The GIA (gross internal area) for each unit is 172m² with the two end units being 178m² and 221m².

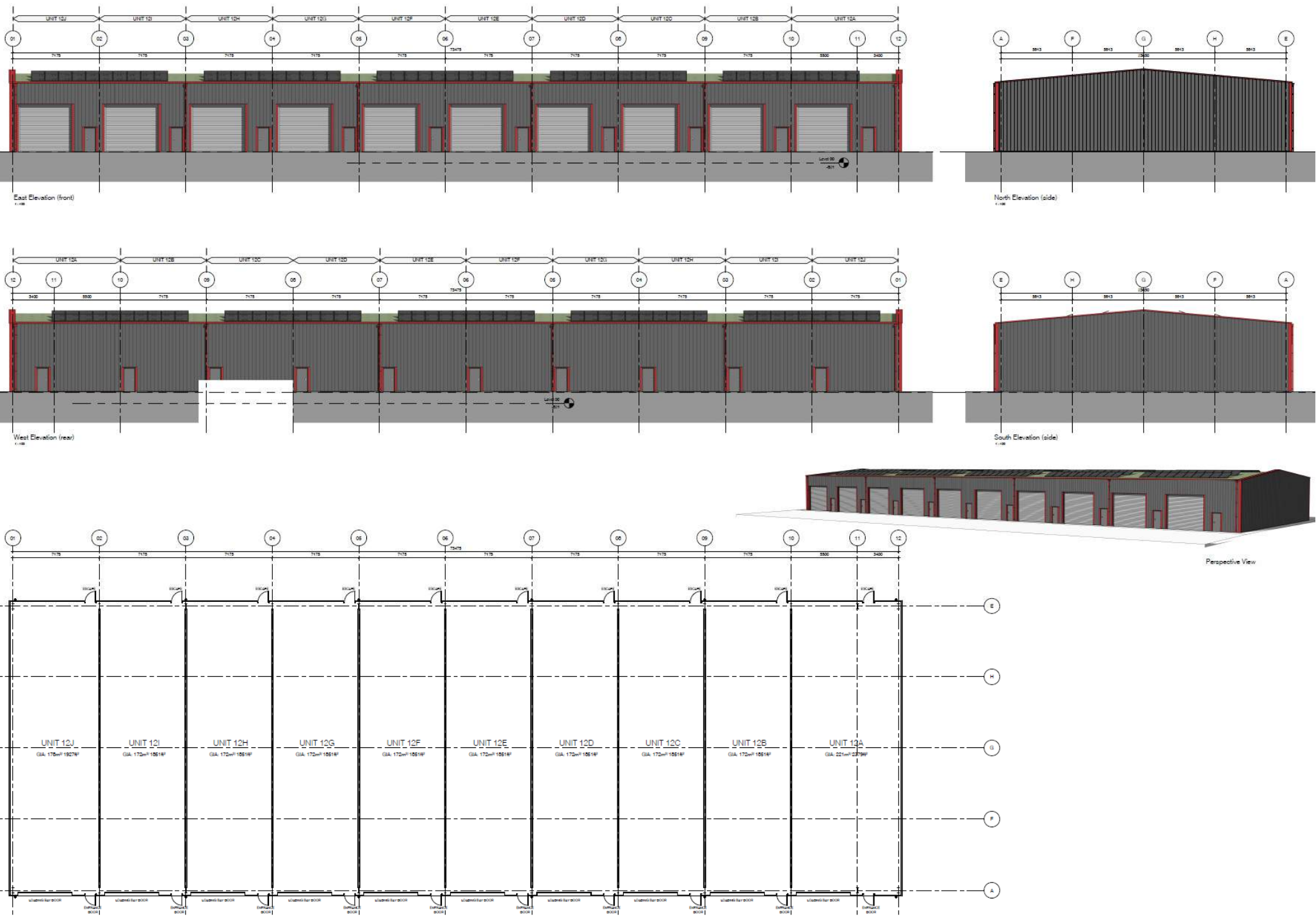
3.3 Final Design Proposals

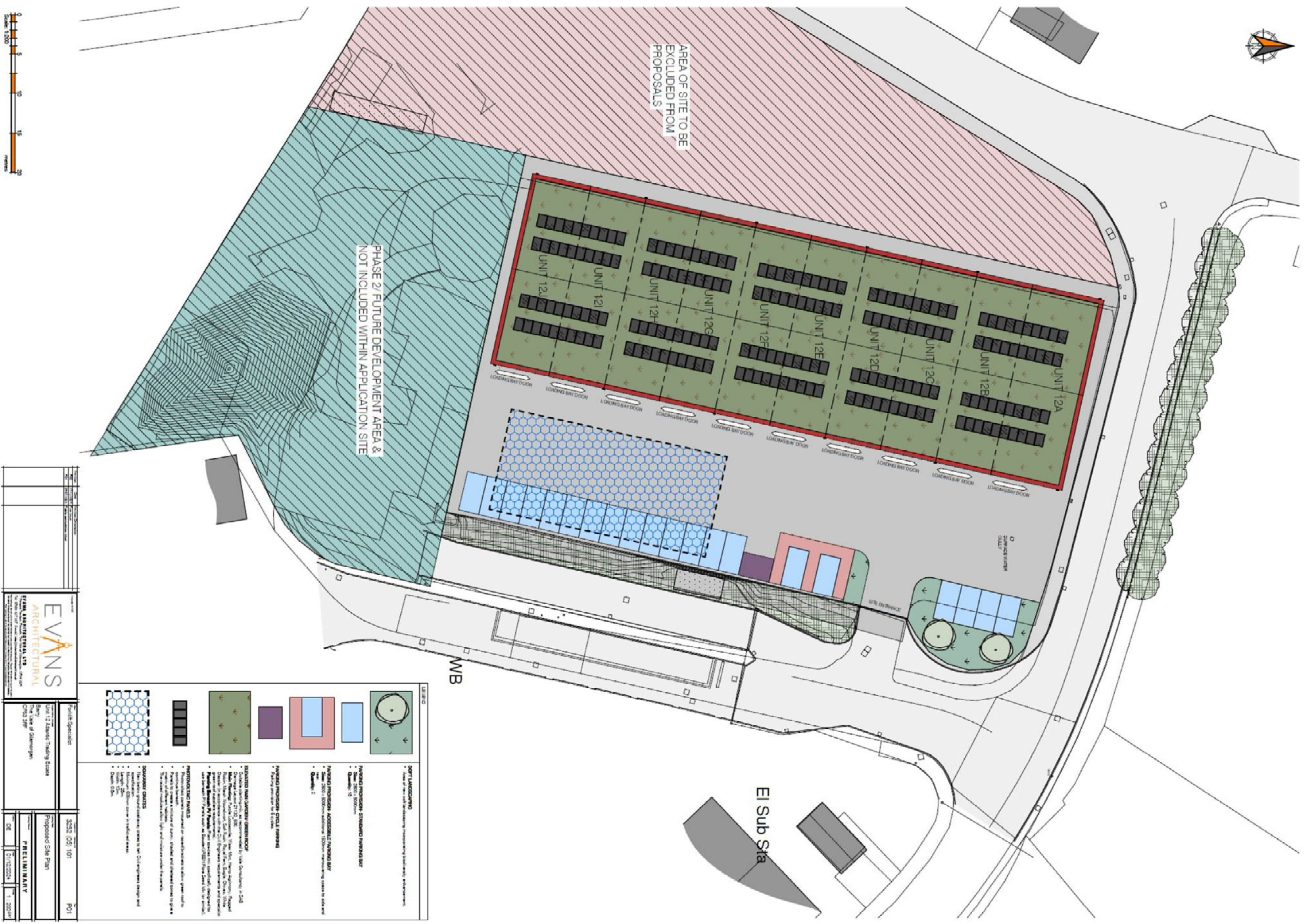
The proposed site plan is shown on one of the following pages (a fully detailed and scaled plan is submitted with the planning application). The final proposal includes 1no unit, split into 10no units of around 170m² GIA each.

The existing shared access to the Resource Recovery Facility from Atlantic Trading Estate will be retained. The minimum width of the access road will be maintained at 11m with the access road to Unit 12 remaining at 6m. Vehicle access within the site will be generally open plan with parking provisions set out.

Parking provision is provided with 2no allocated spaces for each unit, this includes 2no wheelchair accessible spaces.

A Drainage Strategy Report (DSR) and drainage layout has been prepared by Vale Consultancy Civil and Structural Engineers in support of a full SuDS Approval Body (SAB) application for Vale of Glamorgan County Council.





3.4 Landscape and Ecology

An ecological appraisal has not been undertaken as part of this planning application. The site, until recently, was an industrial unit and currently sits vacant awaiting redevelopment. This site contains a slab of concrete and mounds of waste materials awaiting sorting or removal.

Sensitive Lighting Strategy

The external lighting scheme will be designed to be sensitive to the activities of nocturnal animals and will incorporate buffers and cowls, and where possible be installed at low level and directed downwards to prevent upwards and horizontal light spill. Lighting will generally be kept away from the wooded area to the north.

Trees and Hedgerows and Planting

There are no existing trees and hedgerows within the curtilage of the application site.

New areas of planting, as indicated on the proposed site plan that accompanies this application, will consist of planting species of local provenance, indigenous, mixed species hedges or other indigenous trees and shrubs. It is intended to promote and enhance the biodiversity of the site.

Elevated Rain Garden/ Green Roof

As part of the SuDS Approval and Sustainable Drainage, a Bioretention Green Roof will be installed on the proposed unit.

Plants within the roof may include

- Purple Loosestrife
- Water Mint
- Hemp Agrimony
- Ragged Robin
- Marsh Woundwort
- Soft Rush
- Royal Fern
- Bugle
- Chives
- White Dazzler
- Hazel



3.5 Community Safety

If required by the planning authority, consultation will be undertaken with a Police Crime Prevention Advisor for the area and comments from the advisor will be incorporated where feasible and relevant.

The following principles have been considered:

- Public and private routes are well defined with spaces and entrances that provide convenient movement.
- Pedestrian, cycle and vehicle routes are clear, overlooked and well lit.
- The nature, extent and detailed design of boundaries to ensure security but not at the expense of design principles. With the exception of the northern boundary, the existing site boundary is already present and is of the palisade type security fencing. Additional fencing to the northern boundary is proposed and will match existing.

The scheme will have little or no detrimental impact on community safety. The application site is detached from residential areas and will be directly adjacent to sites of similar uses.

CCTV

The installation of CCTV systems will not be incorporated into the scheme, however individual tenants/ occupiers may install individual systems to their individual premises and surrounding areas.

Lighting

The scheme will incorporate lighting which shall be designed to BS 5849 and be positioned to provide an even spread of light over the pedestrian areas and areas generally accessible to the public/ site visitors and occupiers. The lighting scheme will incorporate measures to limit impact on nocturnal animals.

Additional wall mounted external flood lights will be installed above external doors to suit tenant/ occupiers requirements. The lighting strategy will include sensitive lighting strategy measures as recommended within ecology report .

4.0 SUSTAINABILITY

4.0 Sustainability

The proposed development has been designed to minimize impact on the environment and a number of sustainable measures have been considered in the overall design and construction.

4.1 Materials and Construction

The external envelope, including walls and roofs to all new buildings will be designed and constructed to meet, and likely exceed, the minimum requirements of the Building Regulations Approved Document Part L and recommendations of TAN 12, ensuring that current environmental standards are achieved.

4.2 Energy Efficiency

The proposal will seek to contribute energy efficiency and carbon reduction by utilising appropriate plant and building materials. The thermal performance of the buildings shall exceed the minimum standards set out in the Building Regulations to improve the energy efficiency of the buildings.

4.3 Refuse and Recycling

Dedicated waste storage and recycling areas facilities will be provided within the site. Arrangements for collection of waste will be made in accordance with the Local Authority requirements to suit the requirements and needs of individual tenants and businesses.

4.4 Transport

Cycle storage for up to 6no cycles shall be provided within the application site.

4.5 Drainage

As previously mentioned a drainage report and drainage layout has been prepared by Vale Consultancy Civil and Structural Engineers in support of a full SuDS Approval Body (SAB) application for The Vale of Glamorgan County Council.

The proposed SAB Drainage Layout is included with this application.

The proposed SuDS drainage solution is designed to mimic the characteristics of the sites current arrangement as much as possible, whilst also providing a substantial betterment towards the sites overall discharge rate and water quality.

4.6 Photovoltaic Panels

The proposals include roof mounted photovoltaic panels and special elevated mounting brackets to maintain the green roof beneath.

The panels create a mixture of sunny, shaded and sheltered zones to give a matrix of different habitats for a broader range of vegetation whilst also providing refuge areas for small invertebrates from inclement weather.

SURFACE WATER MANHOLE SCHEDULE

MANHOLE REF.	COVER LEVEL (M)	INVERT LEVEL (M)	MANHOLE TYPE	COVER TYPE	DIS MH REF.	SEWER DIA. (MM)	SEWER GRADIENT	SEWER MATERIAL	DEPTH (MM)	MANHOLE DIAMETER
SWIC01	7.850	7.350	TYPE 3	B125	SWIC02	1500	1:150	UPVC	500	250
SWIC02	7.850	7.215	TYPE 4	D400	SWIC03	1500	1:150	UPVC	635	250
SWIC03	7.850	7.025	TYPE 4	D400	SWIC04	1500	1:150	UPVC	825	250
SWIC04	7.850	6.830	TYPE 3	B125	SWIC05	1500	1:150	UPVC	1020	250
SWIC05	8.000	6.710	TYPE 4	D400	SWIC07	1500	1:150	UPVC	1020	250
SWIC06	8.000	6.675	TYPE 4	D400	SWIC05	1500	1:56	UPVC	1325	250
SWIC07	7.975	6.675	TYPE 4 CATCHPIT	D400	SOAKAWAY	1500	1:150	UPVC	1300	250

NOTES:

- ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
- ALL LEVELS ARE SHOWN IN METRES UNLESS NOTED OTHERWISE.
- DO NOT SCALE FROM THE DRAWING. USE FIGURED DIMENSIONS ONLY.
- ANY DISCREPANCIES TO BE REPORTED IMMEDIATELY TO THE ENGINEER.
- THIS DRAWING TO BE READ IN CONJUNCTION WITH ALL RELEVANT ARCHITECTS, ENGINEERS, SUBCONTRACTORS AND SPECIALISTS DRAWINGS AND SPECIFICATIONS.
- EXISTING SERVICES HAVE NOT BEEN SHOWN BUT ARE PRESENT - THE CONTRACTOR IS TO LIAISE WITH ALL STATUTORY AUTHORITIES PRIOR TO THE COMMENCEMENT OF ANY WORKS.

GENERAL DRAINAGE NOTES:

- MH COVERS AND FRAMES TO CONFORM TO BS EN 124:
- HIGHWAYS AND FOOTWAYS TO FRONT OF BUILDINGS (D400)
- FOOTWAYS TO REAR OF BUILDINGS (C250)
- TYPE 3 BEDDING AND SURROUND TO ALL PIPES EXCEPT IN THE FOLLOWING CIRCUMSTANCES, IN WHICH TYPE 2 BEDDING AND SURROUND ARE TO BE USED:
- DOMESTIC GARDENS AND PATHWAYS WITHOUT ANY POSSIBILITY OF VEHICULAR ACCESS, 0.35m;
- DOMESTIC DRIVEWAYS, PARKING AREAS AND YARDS WITH HEIGHT RESTRICTIONS TO PREVENT ENTRY BY VEHICLES WITH A GROSS VEHICLE WEIGHT IN EXCESS OF 7.5 TONNES, 0.5m;
- DOMESTIC DRIVEWAYS, PARKING AREAS AND NARROW STREETS WITHOUT FOOTWAYS (E.G. MEWS DEVELOPMENT) WITH LIMITED ACCESS FOR VEHICLES WITH A GROSS VEHICLE WEIGHT IN EXCESS OF 7.5 TONNES, 0.9m;
- AGRICULTURAL LAND AND PUBLIC OPEN SPACE, 0.9m;
- HIGHWAYS AND PARKING AREAS WITH UNRESTRICTED ACCESS TO VEHICLES WITH A GROSS VEHICLE WEIGHT IN EXCESS OF 7.5 TONNES, 1.2m.
- ALL EXISTING MANHOLE INVERTS TO BE CHECKED AND REPORTED TO THE ENGINEER PRIOR TO THE COMMENCEMENT OF DRAINAGE WORKS. EXISTING PIPE CONNECTIONS ARE TO BE CCTV SURVEYED AND INSPECTED BY THE ENGINEER AND LOCAL AUTHORITY. IF THE PIPE CONNECTION IS FOUND TO BE DAMAGED OR IN DISTRESS, THE CONTRACTOR IS TO CARRY OUT REMEDIAL WORKS OR PROVIDE A NEW CONNECTION TO THE EXISTING SEWER (PIPE SIZE AND GRADIENT TO BE DETERMINED BY THE ENGINEER).
- THE GRADIENTS INDICATED AGAINST THE DRAIN RUNS ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL INSTALL DRAINS TO THE INVERT LEVELS SHOWN FOR EACH MANHOLE OR OTHER INDICATED POSITION.
- ALL PLOT FOUL DRAINAGE TO BE MINIMUM 100mm DIAMETER UNLESS OTHERWISE NOTED.
- ALL ADOPTABLE THERMOPLASTIC STRUCTURED WALL SEWER PIPE SHALL COMPLY WITH THE RELEVANT PROVISIONS OF BS EN 13476-1 AND BS EN 13476-2 OR BS EN 13476-3. PIPES SHALL BE BSI KITEMARKED OR HAVE EQUIVALENT THIRD PARTY CERTIFICATION. PIPES LESS THAN OR EQUAL TO 500mm DIA. SHALL HAVE A SHORT-TERM RING STIFFNESS OF NOT LESS THAN 8kNm² (SN8).
- ALL JOINTING IN ACCORDANCE WITH MANUFACTURERS TECHNICAL ADVICE & SPECIFICATION.
- ALL NON ADOPTABLE DRAINAGE TO BE THE STANDARD UPVC 110mm, 160mm DRAINAGE AND MANUFACTURED TO BS-EN 1401 AND BS 4660 UNLESS NOTED OTHERWISE.
- ALL PERMEABLE PAVING CELLS TO HOUSE LATERAL FIN DRAIN ALONG LOWER EDGE WITH 1500 CONNECTION PIPE.
- ALL STORM WATER DRAINAGE TO BE A MINIMUM OF 150mm DIAMETER UNLESS OTHERWISE NOTED.
- SOIL & VENT PIPE LOCATION AND NUMBER TO BE CONFIRMED WITH ARCHITECT.
- THIS DRAWING TO BE READ IN CONJUNCTION WITH ALL RELEVANT ENGINEERS DRAWINGS, TOGETHER WITH THEIR LATEST SPECIFICATIONS. ANY DISCREPANCIES ARE TO BE REPORTED TO THE ENGINEER (VALE CONSULTANCY) AS SOON AS POSSIBLE.
- FIGURED DIMENSIONS ONLY TO BE USED. ANY QUERIES OR DISCREPANCIES TO BE REFERRED TO THE ENGINEER (VALE CONSULTANCY) IMMEDIATELY.
- PRIOR TO ORDERING ANY MATERIALS THE CONTRACTOR IS TO CONFIRM MANUFACTURER/PIPE MATERIAL AND GRADE PROPOSED WITH WATER AUTHORITY.
- TEMPORARY WORKS DESIGN BY OTHERS.
- DESIGN RISK ASSESSMENTS AND METHOD STATEMENTS ARE TO BE PROVIDED TO THE PRINCIPLE CONTRACTOR PRIOR TO THE COMMENCEMENT OF ANY WORKS.

DRAINAGE LEGEND

- PROPOSED SURFACE WATER DRAINAGE
 PROPOSED RAINWATER PIPE
 PROPOSED SPOT LEVEL
 EXISTING SPOT LEVEL
 DRAINAGE FORMATION LEVEL
 PERFORATED PIPE
 POROUS ASPHALT SERVICE YARD
 POROUS ASPHALT FOOTPATH
 BIORETENTION GREEN ROOF
 SAB BOUNDARY

PLEASE REFER TO DRAWING 21130_501 FOR CONSTRUCTION DETAILS.

PRELIMINARY

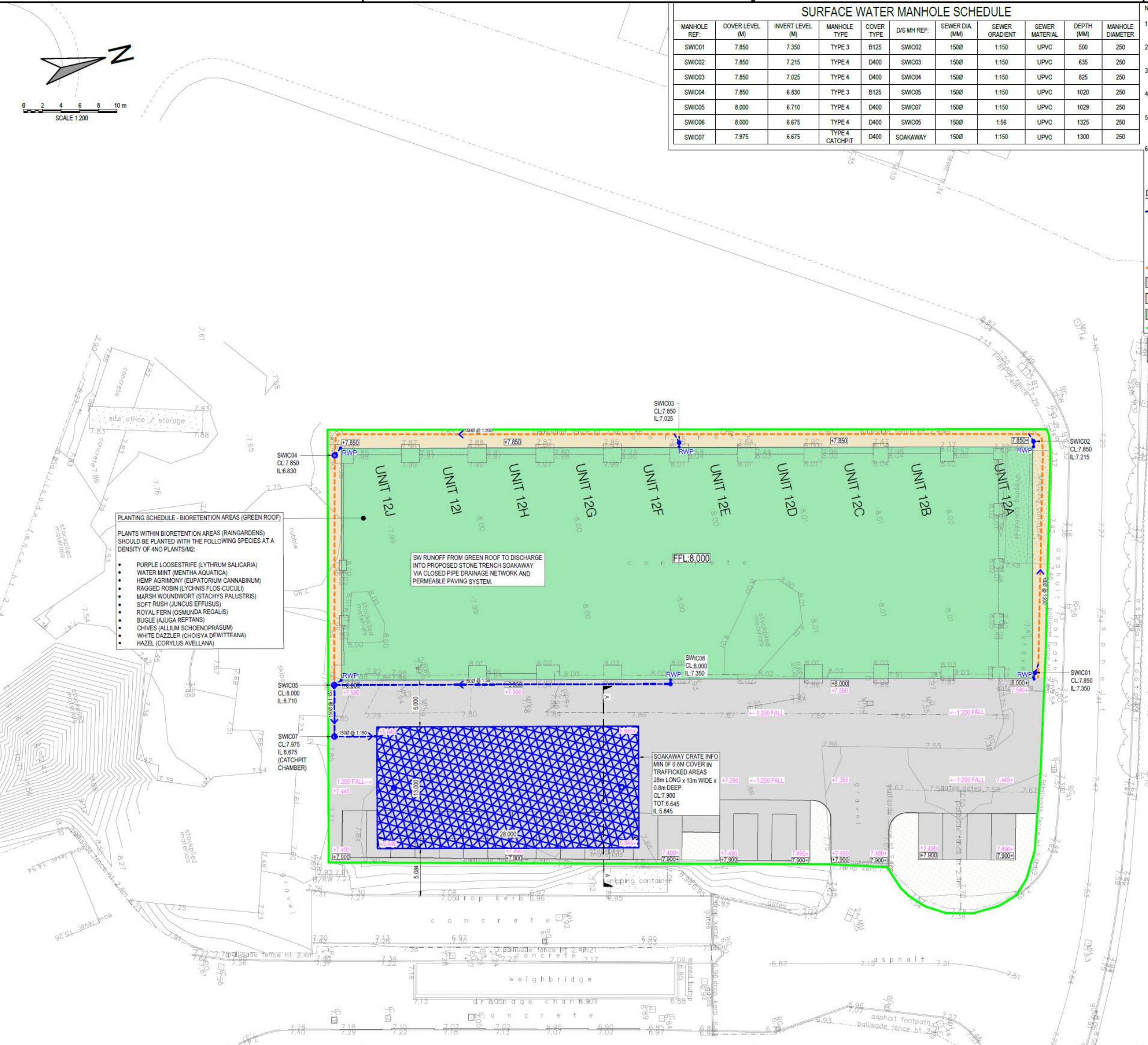
PLANTING SCHEDULE - BIORETENTION AREAS (GREEN ROOF)

PLANTS WITHIN BIORETENTION AREAS (RAINGARDENS) SHOULD BE PLANTED WITH THE FOLLOWING SPECIES AT A DENSITY OF 4NO PLANTS/M²:

- PURPLE LOOSESTRIFE (LYTHRUM SALICARIA)
- WATER MINT (MENTHA AQUATICA)
- HEMP AGRIMONY (EUPATORIUM CANNABINUM)
- RAGGED ROBIN (LYCHNIS FLOS-CUCULI)
- MARSH WOUNDWORT (STACHYS PALUSTRIS)
- SOFT RUSH (JUNCUS EFFUSUS)
- ROYAL FERN (OSMUNDA REGALIS)
- BUGLE (AJUGA REPTANS)
- CHIVES (ALLIUM SCHOENOPRASUM)
- WHITE DAZZLER (CHOISYA DEWITTEANA)
- HAZEL (CORYLUS AVELLANA)

SW RUNOFF FROM GREEN ROOF TO DISCHARGE INTO PROPOSED STONE TRENCH SOAKAWAY VIA CLOSED PIPE DRAINAGE NETWORK AND PERMEABLE PAVING SYSTEM.

SOAKAWAY CRATE INFO
MIN OF 0.6M COVER IN TRAFFICKED AREAS
28m LONG x 13m WIDE x 0.8m DEEP
CL 7.900
TOT 6.645
IL 5.845



5.0 ACCESS AND MOVEMENT

5.0 Access and Movement

The proposal aims to reflect an inclusive approach to access, providing equal access for all users, including those with impaired mobility. The scheme is designed in accordance with Approved Document Part M of the UK Building Regulations.

5.1 Pedestrian and Cycle Access

The site is fully accessible to pedestrians and cyclists. An area of the site has been dedicated to the storage of cycles. Pedestrians can access the site via Hayes Road and the Atlantic Trading Estate internal road network, which has pedestrian footways with dropped kerbs and tactile paving.

5.2 Public Transport

There are four bus stops along Hayes Road, with the nearest to the site being 600m away from entrance.

There are two routes that serve this area

- No.88 Penarth to Barry which frequents every hour
- No.94 Cardiff to Barry which frequents 2 per hour

There are also 4 train stations within a 1.5 mile radius of the site, but due to the surrounding topography there is no direct route. The furthest station- Cadoxton has the shortest route of approx. 3 miles via Hayes Road, Sully Moors Road and then Cardiff Road.

5.3 Private Transport and Parking Provision

The vehicle access from Atlantic Trading Estate is existing and is shared with Resource Recovery Facility. The width of the shared access road shall be maintained at a 11m.

The proposal includes the following parking provision:

- Allocated parking: 2no per unit (20no total)
- Wheelchair accessible visitor parking: 2no (2 of the total 20no spaces)

6.0 CONCLUSION

6.0 Conclusion

This document sets out the detailed design proposals for the development of B2 industrial/ storage unit to replace an existing facility that was damaged in a fire in 2023. The size, scale, character, footprint and position on the site is very similar to the existing. The overall length of the proposed unit is marginally larger to make efficient use of the site with all other differences offering enhancement to biodiversity, and drainage.

The proposal is essential for the applicant who operates a business that supplies and maintains forklifts and is in desperate need for premises. The proposals represent a commitment to providing a sustainable high quality development. The proposal is considered to be policy compliant and is appropriate for the proposed planning use classes.

The proposal is in keeping with the character of the general location and neighboring properties/ businesses and expands upon and already established industrial and business area.

The design, scale, finish and construction of the buildings are typical of similar industrial units and will be in keeping with the character of the area and existing neighboring buildings.

Having regard to the above considerations, it is considered that the proposed development represents an appropriate and compliant scheme, which makes good use of existing brownfield land which will otherwise remain vacant.